



Old Oak Neighbourhood Forum and Grand Union Alliance

November 2nd 2021

On Zoom

Agenda for this session

1. Presentation from Roz Henville (OPDC Head of Communications and Engagement) on the OPDC's updated Community Engagement Strategy
2. Update on OONF application to extend the neighbourhood boundary
3. OPDC Local Plan - our latest submissions to the Planning Inspector
4. Current planning applications and developments
 - 2 Scrubs Lane – planned eviction of City Mission Church/nursery
 - Pocket Living scheme at Atlas Wharf - latest consultation material
 - Cycle way in Old Oak Lane
5. West London Orbital
6. Update on HS2 activities, recent meeting with Transport Minister and Victoria Road crossover box (outcome of FoI request)

OONF application to extend boundary



OPDC consulted between 12th September and 25th October.

OPDC issued letters to all properties within existing and proposed area. OONF used our networks of residents groups.

106 responses. (OPDC had 142 to their Modifications).

Decision by Board 30th Nov following 11th November Planning Committee.

OONF and StQW submissions to Planning Inspector

OPDC consultation on its proposed 'modifications' to its Local Plan ended on 5th July

OONF/StQW and GUA had submitted detailed comments. OPDC lists the former as being 254 separate points

OPDC has accepted only a handful of changes we proposed

The Inspector has issued a note saying he does not wish to hold further examination hearings as took place in 2019

He is legally required to take account of requests for hearings, but identified only 1 wording change requested by OONF/StQW.

Our case for further hearings

We have submitted further arguments for further hearings. The factual position has changed since 2019 and the Inspector may not have followed the Housing Infrastructure Fund saga, for which OPDC has been heavily criticised by the London Assembly

Our 'Issue 1' is that OPDC knew the 19.2 Plan was not '*ready for examination*' when submitted in September 2018.

Our 'Issue 2' is that '*examination*' of the Local Plan has merged with extensive further '*preparation*' in a way that is unlawful.

This may be our last chance to stop this Local Plan from being adopted, and seeing a fresh start on a better plan. The Regulation 18 version was a better plan back in 2016, over five years ago.

Current development proposals and applications

- **2 Scrubs Lane** – 2018 permission for 20 storeys and 85 flats (plus church and nursery) lapsed in June 2021. New proposals for *296 co-living units and community floorspace at ground floor*. City Mission Church eviction notice for November 5th.
- **Pocket Living, Atlas Wharf** – 400 plus units and commercial floorspace at ground floor.
- **Frogmore Industrial Estate** – 33,000 sq metre data centre
- **Mitre Wharf, Scrubs Lane** – application for EIA screening on 435 residential units with heights up to 130m (c. 35 storeys)
- **1 Portal Way and Castle Pub site** – no more news

2 Scrubs Lane

The 20 storey development proposed in 2017 included replacement space for the City Mission Church and day nursery

This accommodation was written into the draft S106 Agreement on this scheme as a 'community benefit'

OPDC Planning Committee granted planning permission October 2017 on this basis

Developers Fruition Properties slow to progress to construction

Started pre-app discussions with OPDC on co-living scheme

Notice of termination of 'tenancy at will' issued for Nov 5th

Andy Slaughter, Wesley Harcourt, OONF trying to intervene.

Pocket Living, Atlas Wharf

Plans for 435 'pocket' homes (studios, small flats) all for rent

Latest plans propose 3 buildings around a landscaped podium

Heights of 8 storeys by canal, and 12 and 28 storey towers

Density 700-750 units /hectare (2016 London Plan max was 405 units at the most 'central' locations with highest level of public transport access)

Construction 2023-5, application 9-12 months away

Waste site to the immediate north of the site

Place Review Group/Community Review Group have looked at the plans

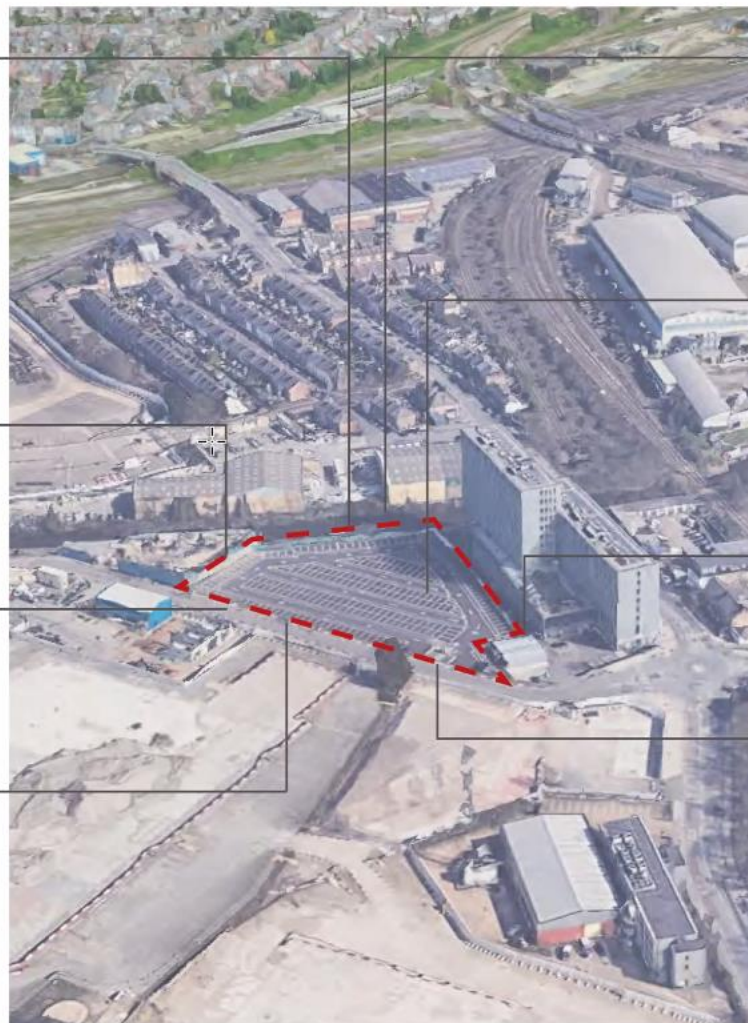
Vision for the Site



GRID
architects

TOWNSHEND
LANDSCAPE
ARCHITECTS

- Working with the CRT to improve the currently hostile and inaccessible canal towpath; to include:
 - widening the canal towpath;
 - introduction of a step-free route;
 - planting, seating and landscaped areas;
 - new routes from Atlas Road.
- Delivering biodiversity net gain on the site with landscaped roofs, podiums and pocket parks.
- Provision of much needed local convenience/amenities for surrounding residents.
- Job creation through the construction and operation of the development.



- Activating the canalside for the benefit of the whole community.
- Delivering circa 435 new high-quality homes for rent.
- Introducing new ground floor uses that will activate and animate the area for the benefit of the community.
- Promoting sustainable travel by providing ample cycle storage and a dedicated cycle workshop.



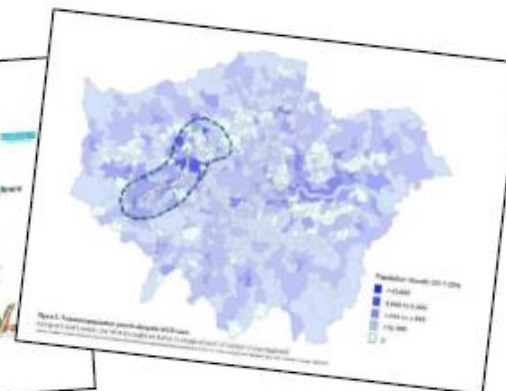
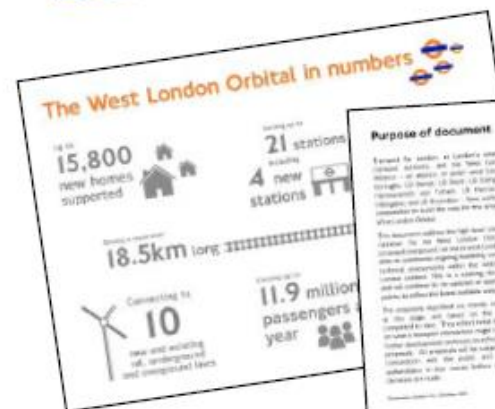
Site Strategy // Developing the massing



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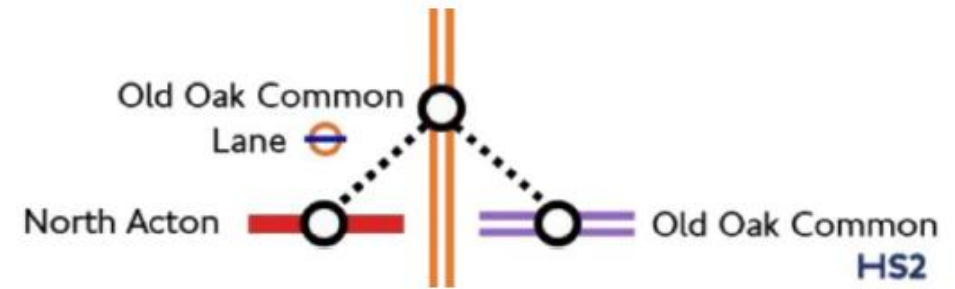
Old Oak Common Lane

HS2 Old Oak Common (HS2)

 Old Oak Common (Elizabeth Line)

 North Acton (Central Line)

Located in LB Ealing, Old Oak Common is also the heart of the Old Oak Park Royal Opportunity Area. It will see Elizabeth Line and HS2 services stop at the new Old Oak Common station to the east of Old Oak Common Lane. Large scale redevelopment is planned for the majority of this area, accommodating thousands of new homes and jobs. The WLO would serve Old Oak Common at a new station located between Victoria Road and Old Oak Common Lane. This would provide on-street interchange with HS2 and Elizabeth Line services at the new Old Oak Common station, just 200m away, as well as Central Line services at nearby North Acton (a short 5-10min walk).



“In current prices, the WLO is expected to cost **between £430m and £610m.**”

1

Developer contributions

Including CIL and Section 106 contributions and levies

2

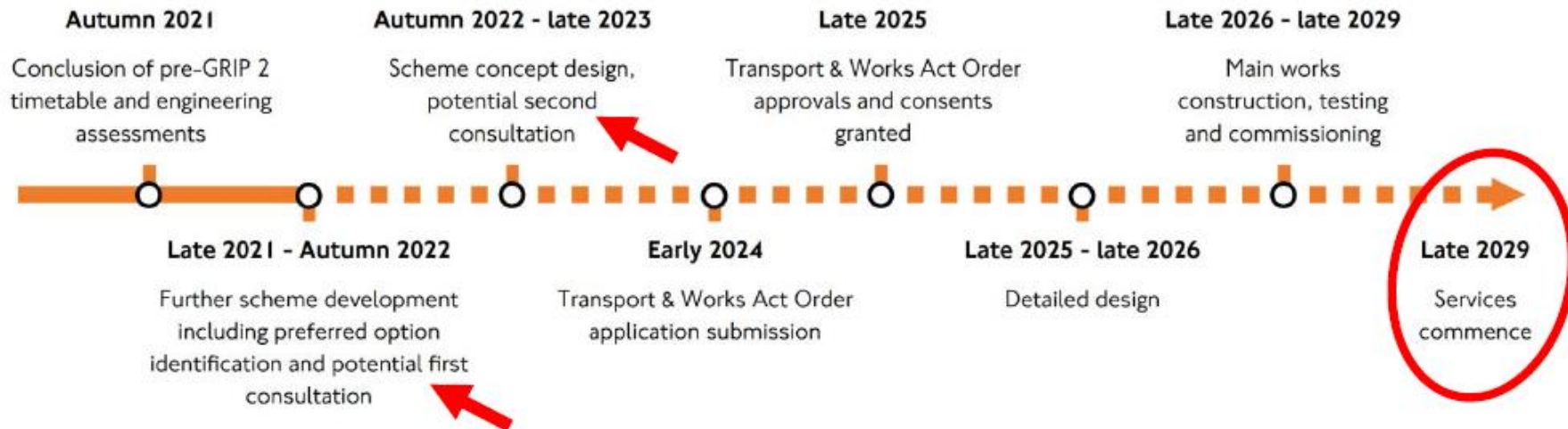
Government grant funding

Targeted funding streams such as those from the MHCLG and the DfT based on specific scheme benefits realised

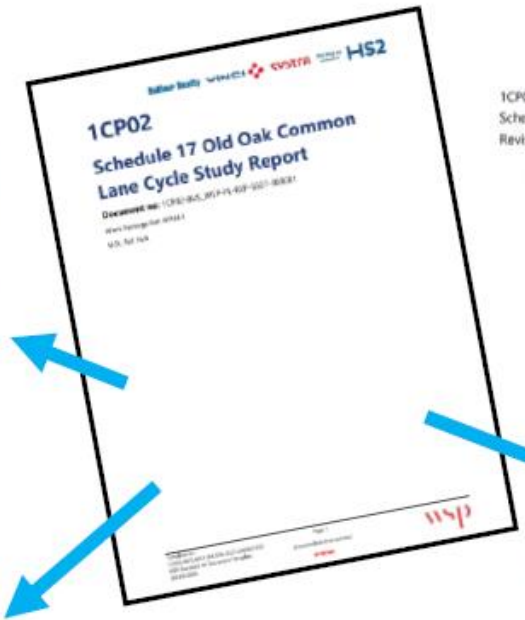
3

Other additional sources

Assessed on a case-by-case basis, such as public sector land development, and business rates and levies



"1.1.5: The cycle lane study has developed through a series of workshops with OPDC, London Borough of Ealing (LBE), London Borough of Hammersmith and Fulham (LBHF), and Transport for London (TfL). Discussions with stakeholders will continue on cycle provision as the highway realignment and wider public realm develops."



- GWML / IEP bridge**
- 6.3.17 The realignment of Old Oak Common Lane is constrained by the existing IEP bridge, which is not modified as part of the HS2 works. The IEP bridge is able to accommodate a 2.5m width footway on the western side and a 3m width footway on the eastern side, allowing for guard rails and other width impingements. This means that the western side does not provide sufficient width for a 3m cycle facility, however the eastern side could accommodate a 3m two-way cycle facility if required.
 - 6.3.18 This constraint has resulted in the potential future segregated cycle route utilising the eastern side beneath the bridge, and would allow for pedestrians on the western side.

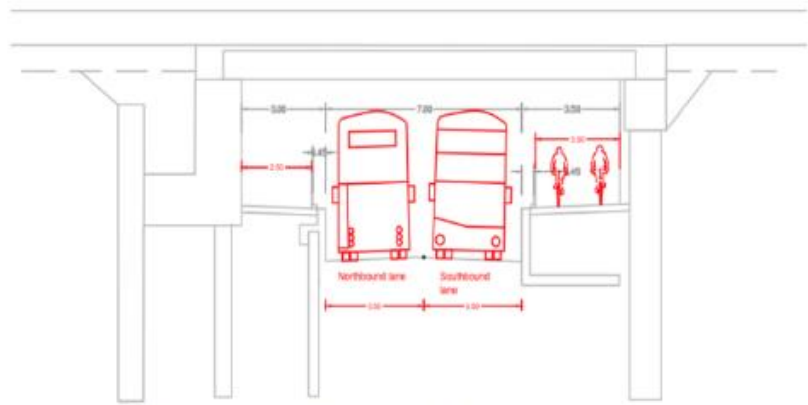
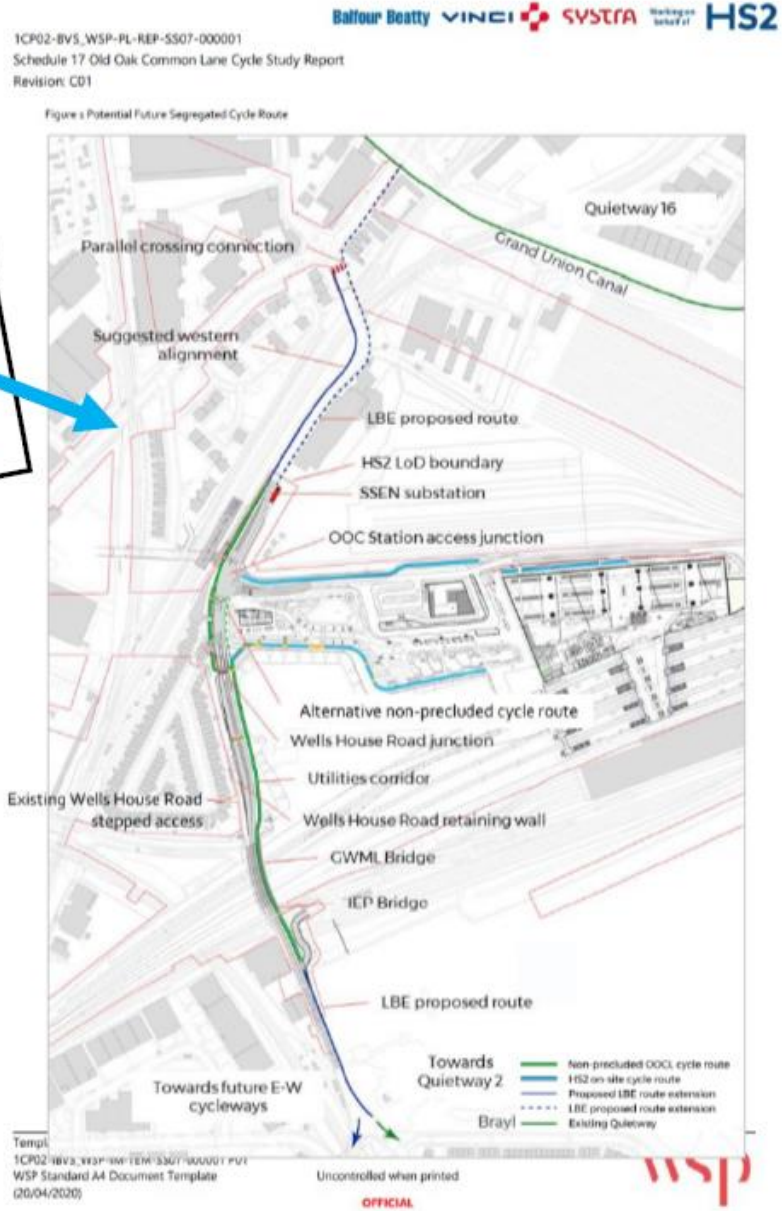


Figure 4 Typical Cross-section at GWML / IEP Bridge (View Looking North)



Cycle Options – High Level Optioneering

- Option 1 – On road (painted)
- Option 2 – 2 way cycle track on eastern footway
- Option 3 – 1 way cycle track on East & West Bound footway
- Option 4 – South bound on road (painted), northbound on footway
- Option 5 - 2 way cycle track on western footway

Constraints;

Bridges Fixed

Footway Retaining Wall Fixed

CRL Depot Retaining Wall fixed

Kerb lines Flexible and can be modified.

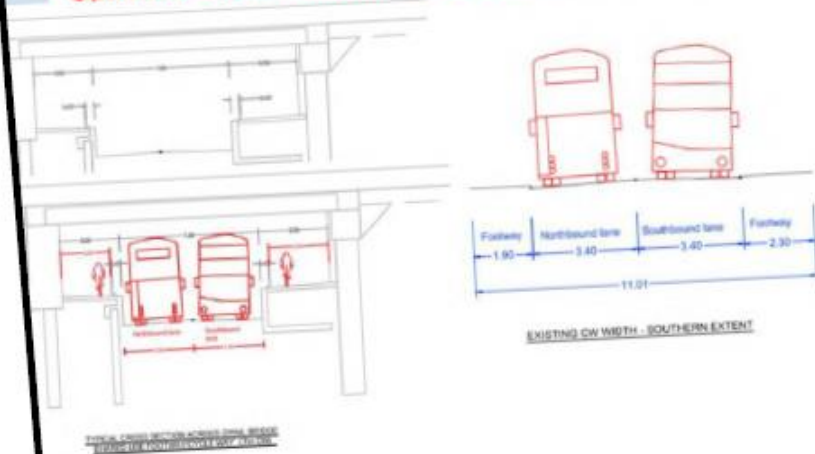


HS2 Old Oak Common

Old Oak Common Lane – Design Update

Document Owner: James O'Regan

Option A – Wells House Road to Southern Scheme Extent



HS2 update and meeting with Transport Minister

Points made in letter to Andrew Stephenson MP following meeting with Rupa Huq MP and residents – report back from Amanda and Ewa:

1. Old Oak/Ealing residents do not have the same level of support as Camden and residents of other impacted areas
2. The sheer volume of impacts and the length of construction in Old Oak Common (10-15 years)
3. Pollution of all kinds is impacting on our communities and is making residents sick
4. Constant noise and vibrations
5. Traffic and Disruption
6. Disrespect of the community by HS2



Does the **RULE OF LAW** operate in **HM Government**?

The UK is a signatory of the 'Aarhus Convention', an international treaty that guarantees public consultation on environmental matters - but HS2 disagrees.



The case for the prosecution:

- p2:* The current finest buildings in Park Royal
- p3:* HS2's December 2020 announcement
- p4:* The HS2 Act application (just as bad)
- p5:* Light at the end of the tunnel
(so to speak)?



Prepared by members of the Park Royal community group 'Grand Union Alliance', and local neighbourhood forums.

(a) PARK ROYAL HERITAGE

as illustrated by 'OPDC Listings'



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Local List

on
the 1930s-40s, the office compo-
is canal facing building is con-
of brown brick with metal windows
(r similar) in Moderne Style. It has
erved strong horizontal elements.

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Road

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Local List

on
e inter-war period, these are a
metrical buildings with stream-
lows, flag poles and Art Deco
o the office components of the
uildings. The buildings are part of a small
group of factories in Elveden Road from the
same period.

(b) HS2 PROPOSALS

'Victoria Rd Crossover Box' site

*from HS2's December 2020 announcement (not consultation),
with the application to be submitted the following month.*

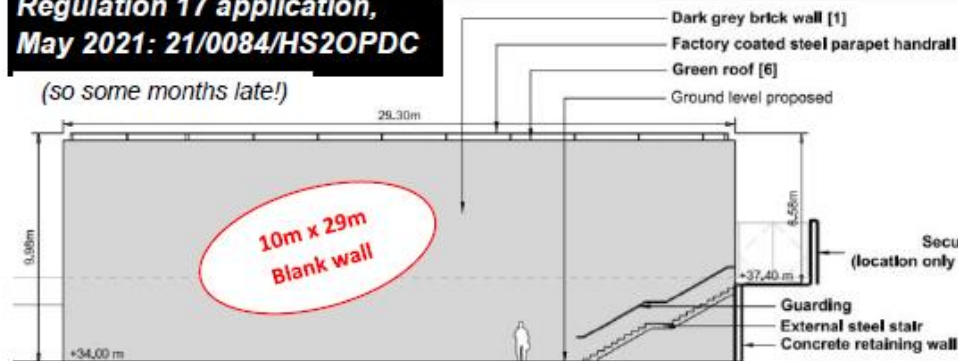


**Can we do better
than this rubbish?**

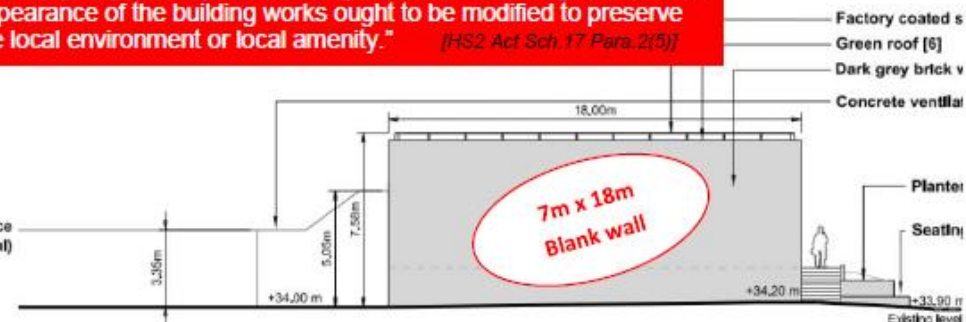


**Regulation 17 application,
May 2021: 21/0084/HS2OPDC**

(so some months later!)

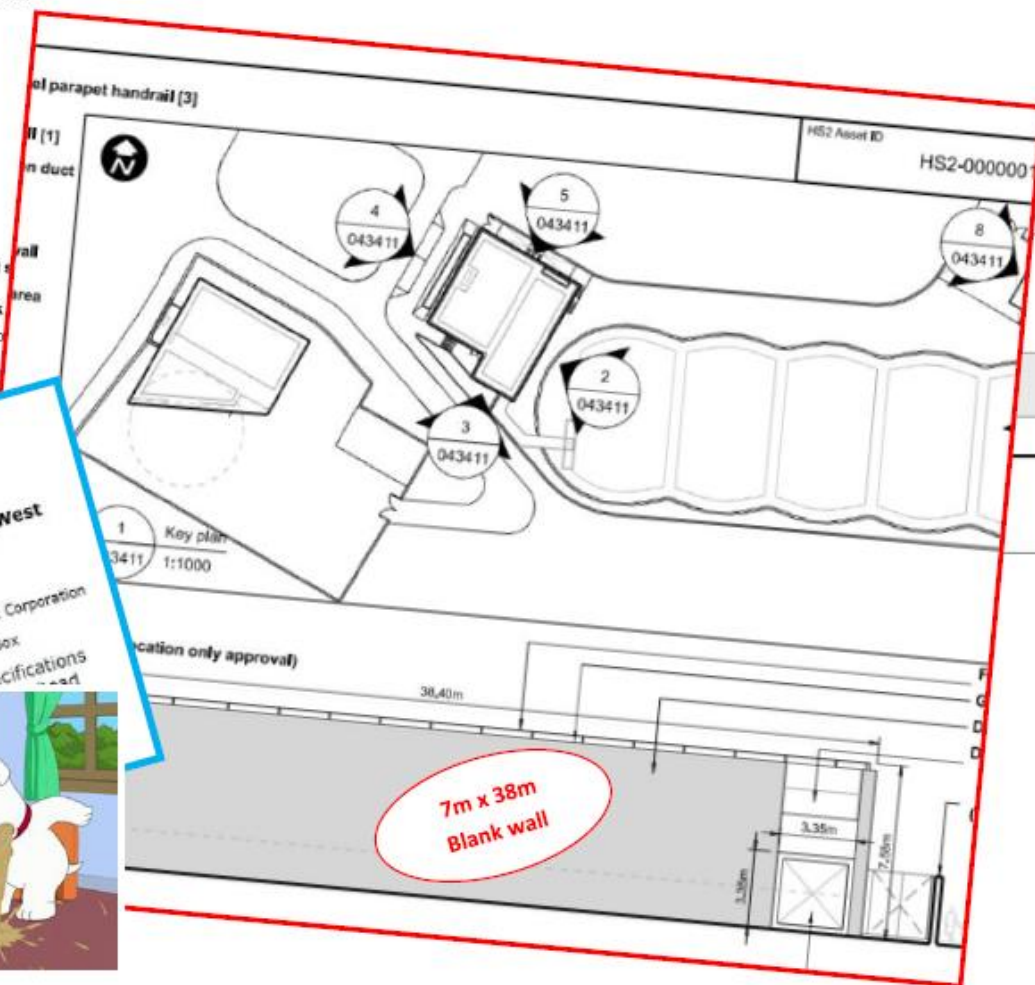
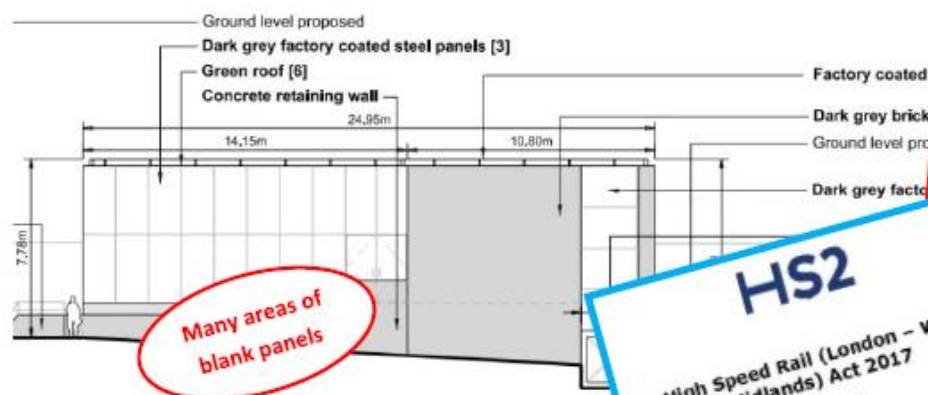


"[OPDC] may refuse plans on the ground that the design or external appearance of the building works ought to be modified to preserve the local environment or local amenity." [HS2 Act Sch.17 Para.2(5)]



x area West Fan Room Building Access gate (for information only)

West fan room



LOAD OF GUFF ALERT!

"A **key design theme** is to reveal, albeit in a **discrete way**, the mechanical nature of the building to **'tell the story'** of its purpose."

"...to form a **hard-edged urban response** with clearly-defined back-of-pavement edges. The buildings are fronted by **metal cladding** to incorporate the more technical functions."



The Old Oak and Park Royal Development Corporation **failed to approve HS2's designs.**

THE OFFICIAL VIEW

In pursuance of the powers under the above Acts, Old Oak and Park Royal Development Corporation has considered your request for approval of plans and specifications for works under paragraphs 2 & 3 of Schedule 17 to the High Speed Rail (London – West Midlands) Act 2017 and **GRANT**

APPROVAL for the works referred to in Part I hereof and as described and shown on the application and plan(s) submitted subject to the following condition(s):

Condition 2:

PRIOR TO INSTALLATION – External Appearance and Finishing Materials: No less than 6 months prior to the commencement of above ground works of the Headhouse, West Fan Room and East Fan Room buildings, details of the external appearance, materials and finishing (including the submission of sample material panels) shall be submitted to and approved in writing by the relevant planning authority. The development shall be carried out in accordance with the approved details. **Reason:** To preserve the local environment and amenity in accordance with Schedule 17 of the High Speed Rail (London – West Midlands) Act 2017.



REALITY

HS2 blinked first, because its unlawful decision not to hold public consultation was unsustainable. Parts of the application were withdrawn just before going to the OPDC Planning Committee.

Still no acknowledgement it must carry out lawful consultation!

HS2 insists that it will not carry out public consultation because it is '**not required**' under the High Speed Rail (London - West Midlands) Act 2017.

But 'health and safety' is not mentioned in it either, and HS2 is not lawfully able to kill a couple of construction workers every day!

The Park Royal community demands:

HS2 must acknowledge the Aarhus Convention is embedded in UK law (it is a United Nations treaty, not an EU one) and therefore it **MUST** carry out public consultation of a standard that would be upheld as 'reasonable' under English common law.

We do not want HS2 random management decisions, we want lawfulness acknowledged. Only then can we examine what public consultation is offered or not offered to the public.

After all, no Minister of the Crown would ever suggest breaking an international treaty, would they?

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ActOne
cinema

To end with some
good news.....

Any other business

Contact details for OONF

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